

Honorary Patron of EUSI – Her Honor, Lois Mitchell, Lieutenant Governor of Alberta



EDMONTON UNITED SERVICES INSTITUTE

PRESIDENT'S ENEWS

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President's Christmas Greeting

Friends and supporters of the EUSI, I hope you all have had a great 2017 and in good health. Long term weather forecast suggested Canada, especially the prairie region will have a long and cold winter. You will need to be in good shape to shovel all that white fluffy stuff. For you all skiers, may next year never see a flick of snow till after Christmas. And as politically incorrect as I am, I am not afraid to use the word Jesus and Christmas and wish you all a very Merry Christmas and a Happy New Year. For our brothers and sisters of the Canadian Forces who are away from home and cannot be with their family during this time of year, I thank you for your sacrifices and May God watch over you.

Please drink responsibly and do not drink excessively and then drive.



Remembrance Day Service

As usual, a number of EUSI members attended services throughout the city and surrounding areas, laying wreath on behalf of their respective organizations. Capt John Pinsent and I laid a wreath on behalf of EUSI at the Aviation Museum. The Remembrance Day Levee held at HMCNONSUCH was attended by well over 250 guests. Commodore Dan MacKeigan, Naval Flag Community Outreach Officer was a special guest and helped cut a cake baked by the sailors. A plaque thanking her service to the military and to EUSI was presented to Nicole Goehring, Provincial Military Representative, MLA Castledowns.



2018 Symposium –

As our world order and security is increasingly threatened with nuclear attacks, Canada must begin to ask if we are able to defend our homeland. The Deputy Commander of NORAD, a Canadian Air Force Lieutenant General recently advised the Senate Committee on National Security and Defence that the USA is not obliged to defend Canada in the case of a nuclear attack. The difficulty of co-dependence between the USA and Canada began when then Prime Minister Diefenbaker refused to act on American request of mobilizing the air force during the Cuban Missile Crisis and Canadian refusal to comply with a US request of a naval blockade.

I read some time ago that missile defence is like hitting a bullet with another bullet in mid air. It is a precise weapon system that Canada has yet to acknowledge it is required.

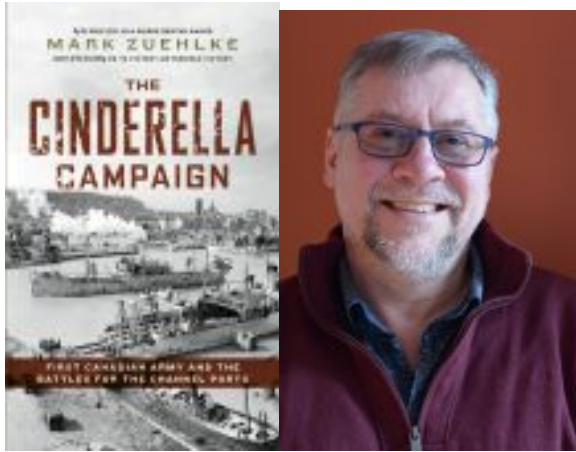
The 2018 symposium will explore this topic and I hope to invite a number of defence experts. If any EUSI member has a suggestion or opinion, please advise the president of EUSI. A board meeting is planned for January. All members are welcome to attend and offer your suggestions.

Humor



THE CINDERELLA CAMPAIGN BY Mark Zuehlke, the 2014 Pierre Berton Award!

First Canadian Army and the Battles for the Channel Ports



The story of how First Canadian Army opened the way to the Allied victory in World War II, in the twelfth installment of the bestselling Canadian Battle Series.

They thought of themselves as the "Cinderella Army," and international correspondents agreed. This was because First Canadian Army had been relegated to the left flank of the Allied advance toward Germany from the Normandy beaches and given the tough, thankless task of opening the Channel ports from Le Havre to Ostend in Belgium. Then suddenly in early September 1944, securing these ports became an Allied priority, as this would allow Field Marshal Montgomery to drive to the Rhine with Operation Market Garden and win the war before Christmas.

Given only scant access to the Allied supply chain, the Canadians and their British partners in I Corps tackled the task assigned. Just getting to the ports proved a terrific undertaking fought against brutal German resistance. And once there, they faced fortresses that had been prepared for years to defeat an attack. "Lost outposts," the Allies called them, but the Germans within were not going to give up easily. And so over the month of September, the Canadians set about fighting for control of each port, scrambling for supplies while under constant military pressure to get those ports open *now*. For Canada this was the Cinderella Campaign, the battle for the Channel ports. For those who fought it, the sacrifice of comrades dead and wounded would never be forgotten.



ANADIAN GEAR CC-138 Twin Otter



The highly adaptable CC-138 Twin Otter is well suited for Canada's ever-changing northern climate and terrain. This short takeoff and landing (STOL) utility transport aircraft can land on water, land, snow and ice. Powered by twin turboprop engines, the Twin Otter is highly maneuverable and has a service ceiling of over 8000 meters. It can be outfitted with wheels, skis or floats to land on virtually any surface. The CC-138 is used in search and rescue (SAR) missions, and transport and support roles to the Canadian Forces' northern operations. It can carry up to 20 passengers or 2999 kg of payload, and has a range of 1427 km. The Canadian Force's 4 Twin Otters are based in Yellowknife, NWT.



A bit of Military History Pearl Harbour, December 7 1941

Seventy-one years ago, Japanese aircraft slashed through the morning skies over Pearl Harbor Naval Station, Ford Island Naval Air Station, Hickam Field Army Air Corps Station, and Wheeler Field and the Schofield Barracks Army Station on the northwest side of Oahu.

In human lives, the attack on Pearl Harbor was horrific. 2,403 were dead, and 1,178 wounded.

188 planes were destroyed, the vast majority on the ground, as only a few Army Air Corps fighters managed to get airborne. A further 159 aircraft were significantly damaged, leaving only 43 planes operational at attack's end.

It was the toll in ships that was staggering, however.

"Battleships

- **Arizona** blown up with a loss of 1,177 men.
- **Oklahoma** capsized with a small part of her hull above water.
- **California** "sank gradually for about three or four days: and came to rest rather solidly on a mud bottom, with her mainmasts and the upper parts of her main batteries above water. "The quarterdeck [was] under about twelve feet of water..."
- **Nevada**, which had got under way, beached in the narrow channel opposite Hospital Point in a wrecked condition.

West Virginia sunk at her berth.

- **Maryland** moderately damaged but not needing to go into drydock.
- **Tennessee**, seriously damaged aft in the officers' quarters from fire and otherwise moderately damaged.
- **Pennsylvania**, in drydock, with considerable damages, "but not of vital nature."
- **Utah**, then used as a target ship, capsized, having been at the **Saratoga's** regular berth.

Light Cruisers

- **Raleigh**, **Helena**, and **Honolulu** moderately damaged.

Destroyers

- **Cassin** and **Downes**, in Drydock No. 1, severely damaged.
- **Shaw's** bow blown off while in floating drydock, severely damaged.

Others

- **Vestal** (repair ship) was alongside the Arizona when the raid commenced and was beached at Aiea to prevent further sinkage.
- **Curtiss** (seaplane tender) was badly damaged by a crashing plane and one 500-lb. bomb.
- **Oglala** (minelayer) capsized.**

For the Japanese, the cost was minimal.

"Twenty-nine planes did not return: fifteen dive bombers and high-level bombers, five torpedo planes, and nine fighter escorts. The midget submarines inflicted no damage, and none returned to their mother ships; four were sunk, and one was wrecked on a reef, its captain captured. One I-class submarine was also sunk."*

The Japanese sneak attack catapulted the isolationist American nation to a Declaration of War, made by Congress the following day, at the request of President Franklin Delano Roosevelt in his stirring "Day of Infamy" speech.

The rest of the Story

Amazingly, of the twenty ships mentioned above, which indeed are the ones that received any damage of a nature greater than superficial, only **Arizona**, **Utah**, and **Oklahoma** were not raised, repaired and returned to wartime service. And **Utah** was little more than a hulk to begin with. Ultimately, one of the real stories about Pearl Harbor is this superb salvage effort to get the ships repaired well enough for a voyage to a West Coast shipyard, where they were repaired and in many cases overhauled and modernized, often returning to service in much finer condition than prior to the attack. The men and women who performed these tasks at Pearl Harbor are as big a set of heroes as any crew who sailed their ships against the Japanese in the Pacific.

All the ships served with distinction later in the war, and it was a fitting event at the Battle of Surigao Strait when Admiral Jesse Oldendorf led six U.S. Battleships, among them Pearl Harbor veterans **California**, **West Virginia**, **Maryland**, **Tennessee** and **Pennsylvania** in a classic "Crossing the T" maneuver, just as Togo had done at Tsushima Strait in 1905, and sank most of Vice Admiral Nishimura's striking force of battleships and cruisers. Oldendorf's victory at Surigao Strait is a testament to that magnificent salvage effort.

The salvage work done at Pearl Harbor in the aftermath of the December 7th attack was finely managed and heroically carried out. Icing to the cake was added barely six months after the Japanese attack when the Naval Shipyard located at Pearl completed what would normally have taken several months to repair: the battle damage to the **USS Yorktown** from the Battle of Coral Sea, in 48 hours, allowing her and her aircrews to

participate in the first major naval victory against the Japanese at the Battle of Midway. Aircraft from the three US aircraft carriers, the **Hornet**, **Enterprise**, and **Yorktown**, the ones that were missed at Pearl, sank four of the Japanese aircraft carriers that participated in the December 7th attack on Pearl Harbor, the **Hiryu**, **Soryu**, **Kaga** and **Akagi**.



New Mental Health and Rehabilitation Center for Canada's Veterans and First Responders Proposed for Surrey, BC

A proposed rehabilitation centre, with an architectural design inspired by the Canadian National Vimy Ridge Memorial in France, is set to become the first of its kind in Canada – a centralized facility that provide physical rehabilitation, PTSD, and mental health treatment for veterans of the CAF and first responders. The Royal Canadian Legion BC/Yukon Command in partnership with local developer Lark group have submitted an application to the City of Surrey to redevelop the current site of the Whalley Legion Branch 229 at 13525 – 106 Avenue and legion Veterans Village. If all goes ass planned with City approval, construction of the Veterans Village, estimated to cost \$60 million, could begin in spring or early summer 2018 for a completion date of 2020.

Securitas

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IT IS TOO LATE TO SHARPEN SWORDS, WHEN THE DRUMS OF WAR ARE BEATING